

Montour No. 9 Mine (Bob Ciminell ©2012)

Montour No. 9 mine was opened by the Pittsburgh Coal Company sometime after 1914 and the completion of the Montour Railroad Mifflin Extension. It was located at McAdams, northwest of McDonald, along PA Route 980. The main entries passed beneath Route 980 and were oriented to the northeast and southwest, with side entries aligned northwest and southeast. Another set of entries passed beneath the railroad tracks; however, that area was not extensively mined because it abutted the Shaw Mine north of Midway.

The Pittsburgh seam at Montour No. 9 was 1,100 feet above sea level, rising toward the northwest and dipping toward the southwest. The surface elevation at the intersection of Route 980 and North Branch Road is 1,104 feet above sea level, which meant that No. 9 entered the seam as soon as it went under Route 980 and drainage from the mine went directly into the tributary of Robinson Run that parallels Route 980.

Montour No. 9 was a large mine, as evidenced by the employment numbers in the table below. The Keystone coal report says the mine closed in June 1954, but other records indicate that deep mining ended in 1946 and stripping ended in 1947. I think they may have confused No. 9 with the Shaw Mine on the other side of the hill.

During its peak years of 1932 and 1934, the mine loaded about 2,600 tons a day, or approximately 50 cars a day, for the Montour Railroad to haul back west to Champion for cleaning and sizing.

Production records for Montour No. 9 are as follows:

Year	Tonnage	Days Worked	Employees	Equipment
1930	3,000 tons per day			Mechanical screens, Picking tables, Booms
1931	411,901 tons	151	635	
1932	655,702 tons	229	768	
1934	636,660 tons	234	686	
1944	469,659 tons	304	333	
1945	372,563 tons			
1946	256,969 tons			
1947	1,000 tons per day		125	11 mining machines, 6 loading machines, 7 trolleys, 800 mine cars, 22 miles of track
1954	77,013 tons			

Montour No. 9 had a much checkered history. It was closed in 1925, reopened, closed indefinitely in 1931, reopened in 1933, was taken over by the Government in 1943, and furloughed 800 men in 1951.

Maps, photos and other images describing Montour No. 9 are located below.

This 2010 Google Earth image shows the former site of Montour No. 9 and the miners' village at McAdams. The pushpin shows the location of the main entries that passed beneath Route 980. The level area between the highway and the Montour Trail was the location of the tippie and yard.



This 1939 aerial photo show the tipple and waste dump for Montour No. 9 and the village to the west on Route 980. Note the bridge across the Montour Railroad northwest of the tipple.



The tipple is gone in this 1956 photo and a major stripping operation is underway northwest and southwest of the mine. Note that the bridge over the Montour tracks is also gone. There is a string of empty hopper cars in the lower left corner of the photo, but no evidence of where they would be loaded.



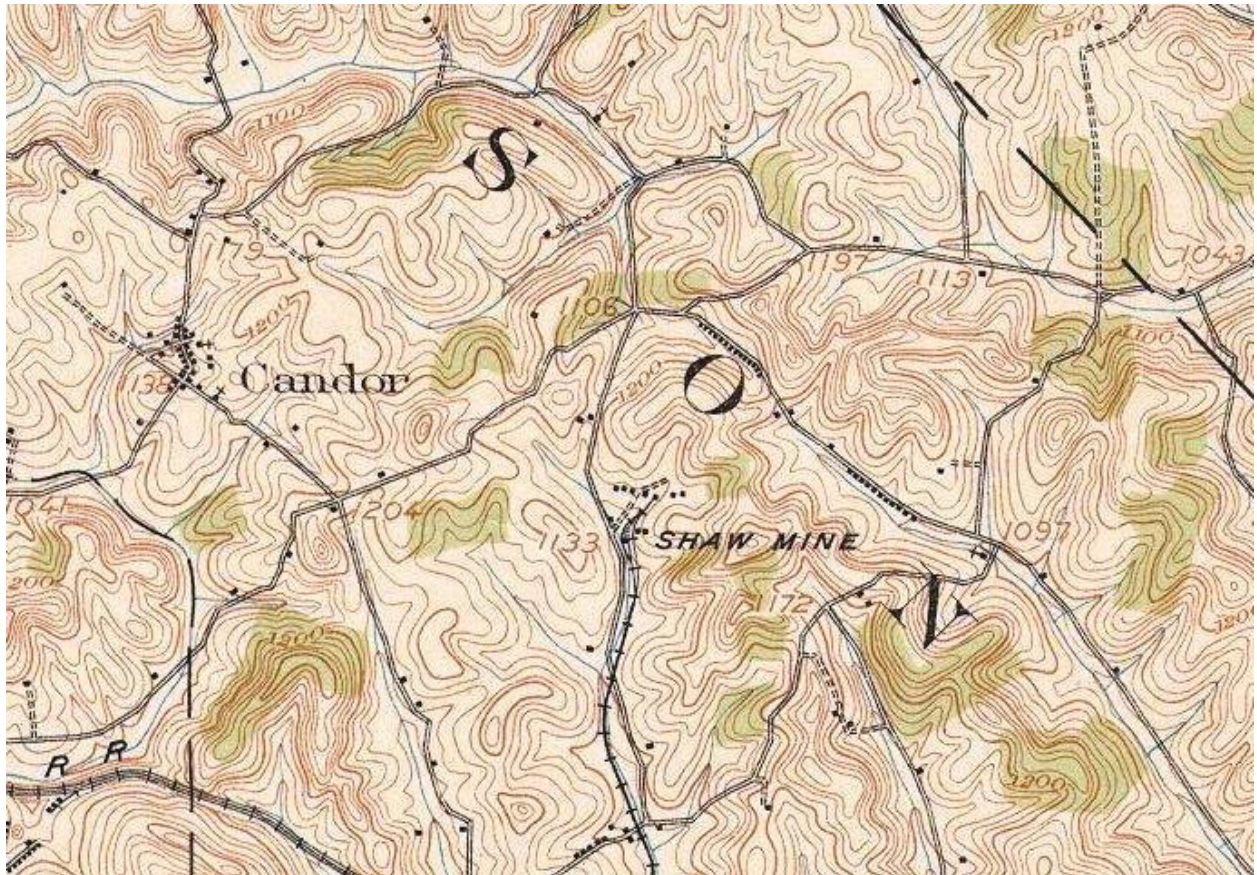
By 1967 some of the old strip mine has been reclaimed and a new light manufacturing facility, Pittsburgh Processing Company, has located at the former mine site.



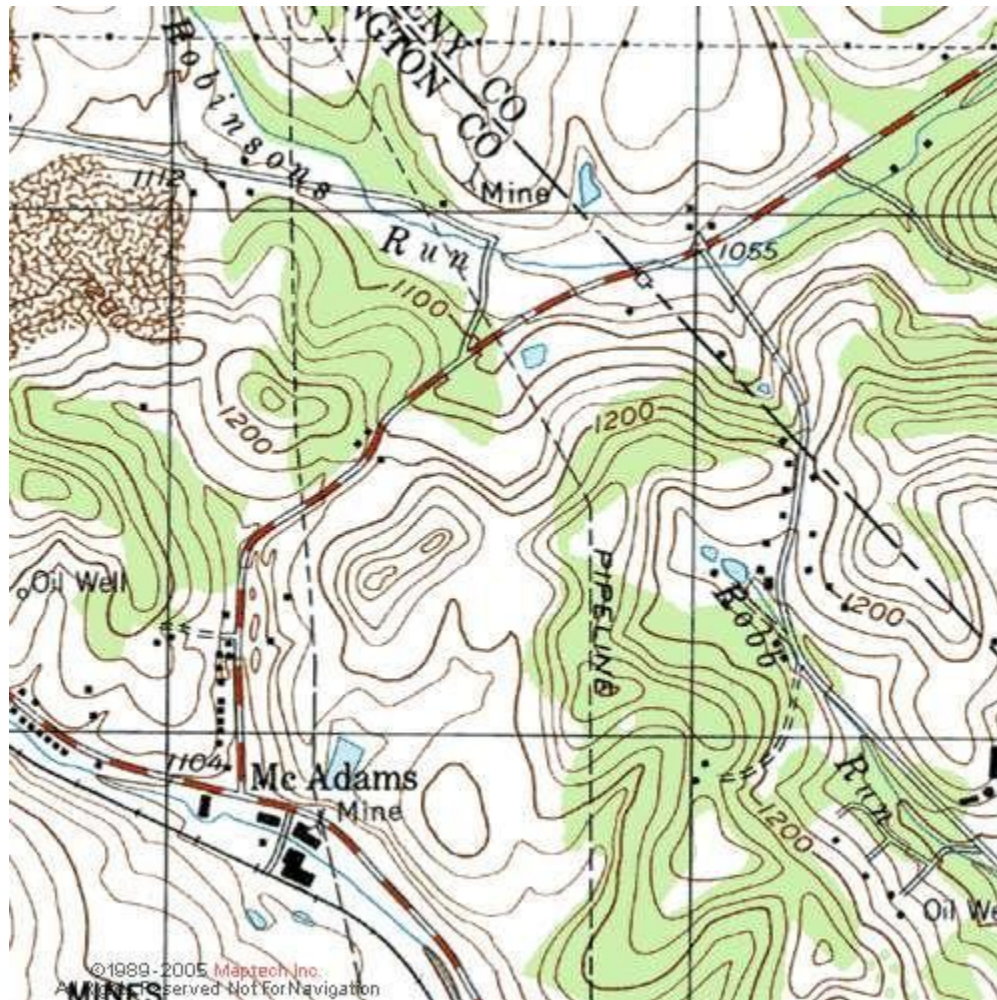
This 1973 aerial photo is taken from a different angle with the view toward the northwest. The McAdams miners' patch is at upper center and the old mine site is in the lower center. The former strip mine highwalls and the waste dump dominate the scene.



This 1906 topographic map shows miners' houses along Route 980 east of Shaw Mine, but no sign of Montour No. 9. This may be an indication that there was an entry, or multiple entries, into the Shaw Mine near the highway. The PRR Panhandle Division's Midway & Oakdale Branch can be seen going up to the tipple at Shaw Mine. The rail spur was originally built by the Shaw Coal Company but was taken over by the PRR when the Pittsburgh Coal Company leased the Shaw operation.



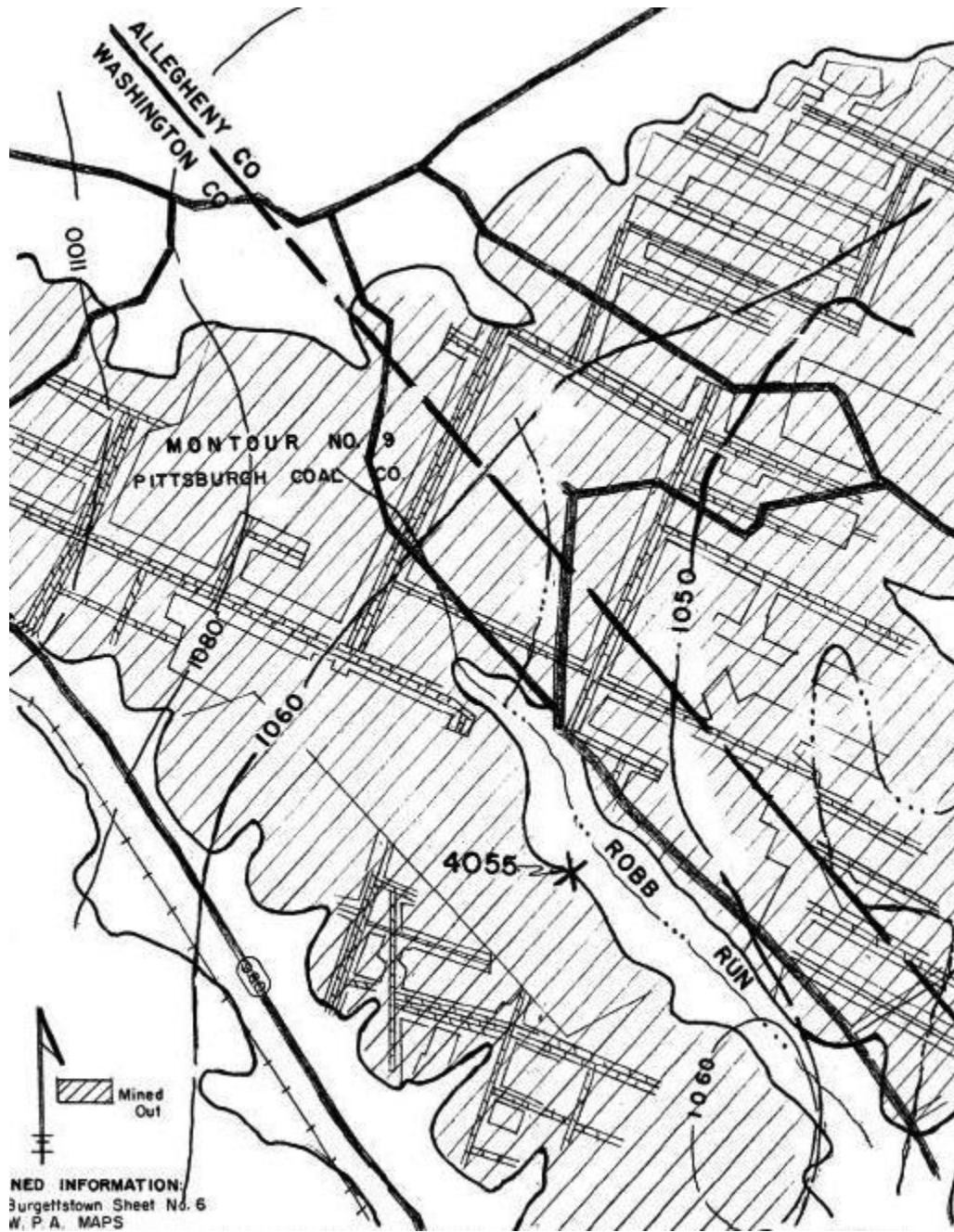
A more recent topographic map, which has obviously not been updated since the Montour Railroad was abandoned, shows the old entries for Montour No. 9. The mine entry shown to the north below the "O" in Washington County was the point where Montour No. 9 exited the seam to the southwest of Robinsons Run and then re-entered it on the other side of the road. The next photo shows the mine tracks crossing the road at this location.



This close-up shows the trestle where the mine trains for Montour No. 9 crossed Donaldson Road to enter the second hill. The entry to the southwest became a major source of acid mine drainage as shown in the next photo for the Operation Scarlift Report for Robinson Run.



This map shows the drainage point from Montour No. 9 where the old mine trestle crossed Donaldson Road and Robb Run (Point 4055)



This photo shows the Montour No. 9 tipple. We are looking railroad west toward Champion. Note the bridge crossing the tracks in the background. The loaded hopper car is a PRR car and may or may not go back to Champion for cleaning.

